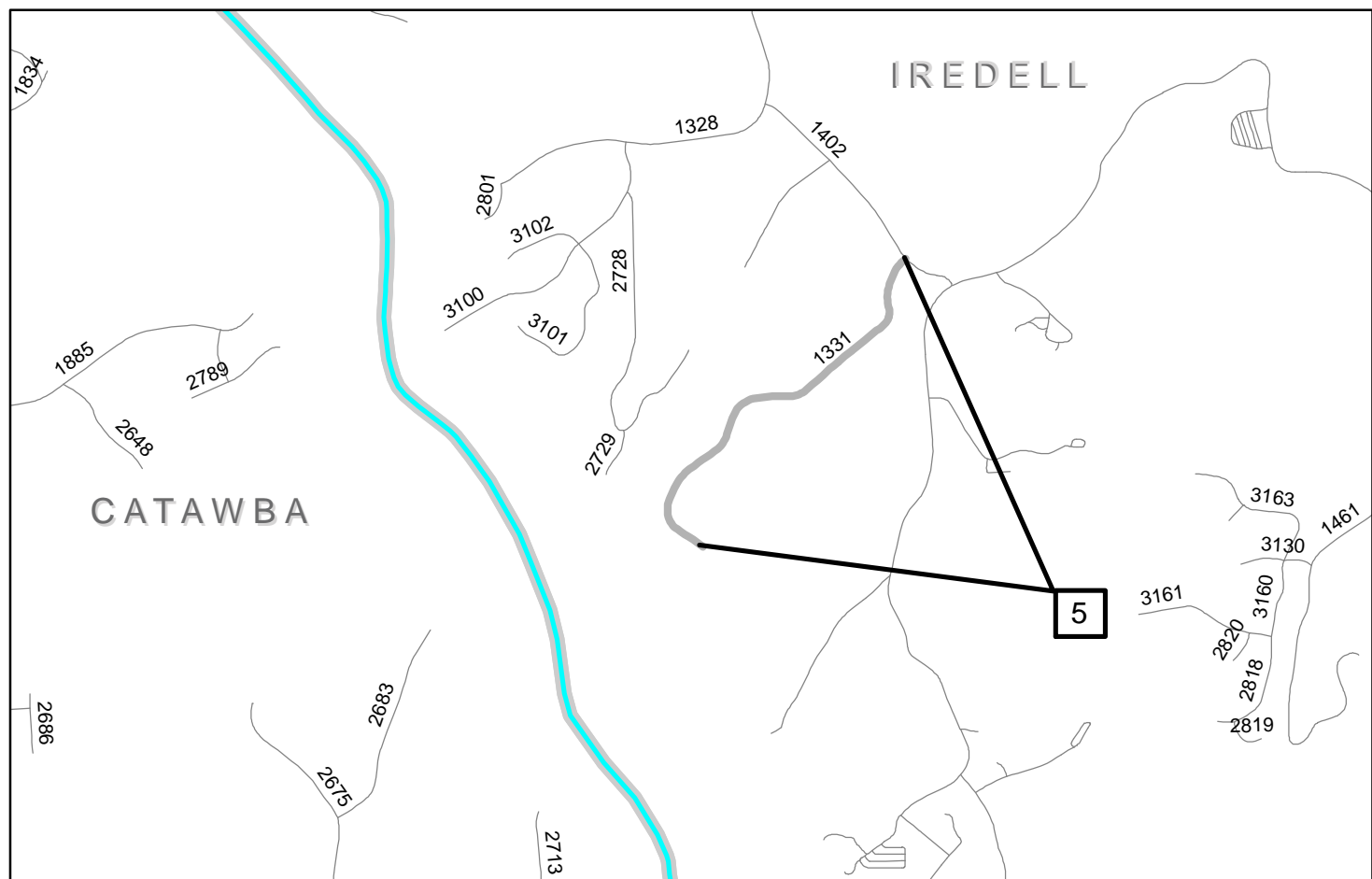
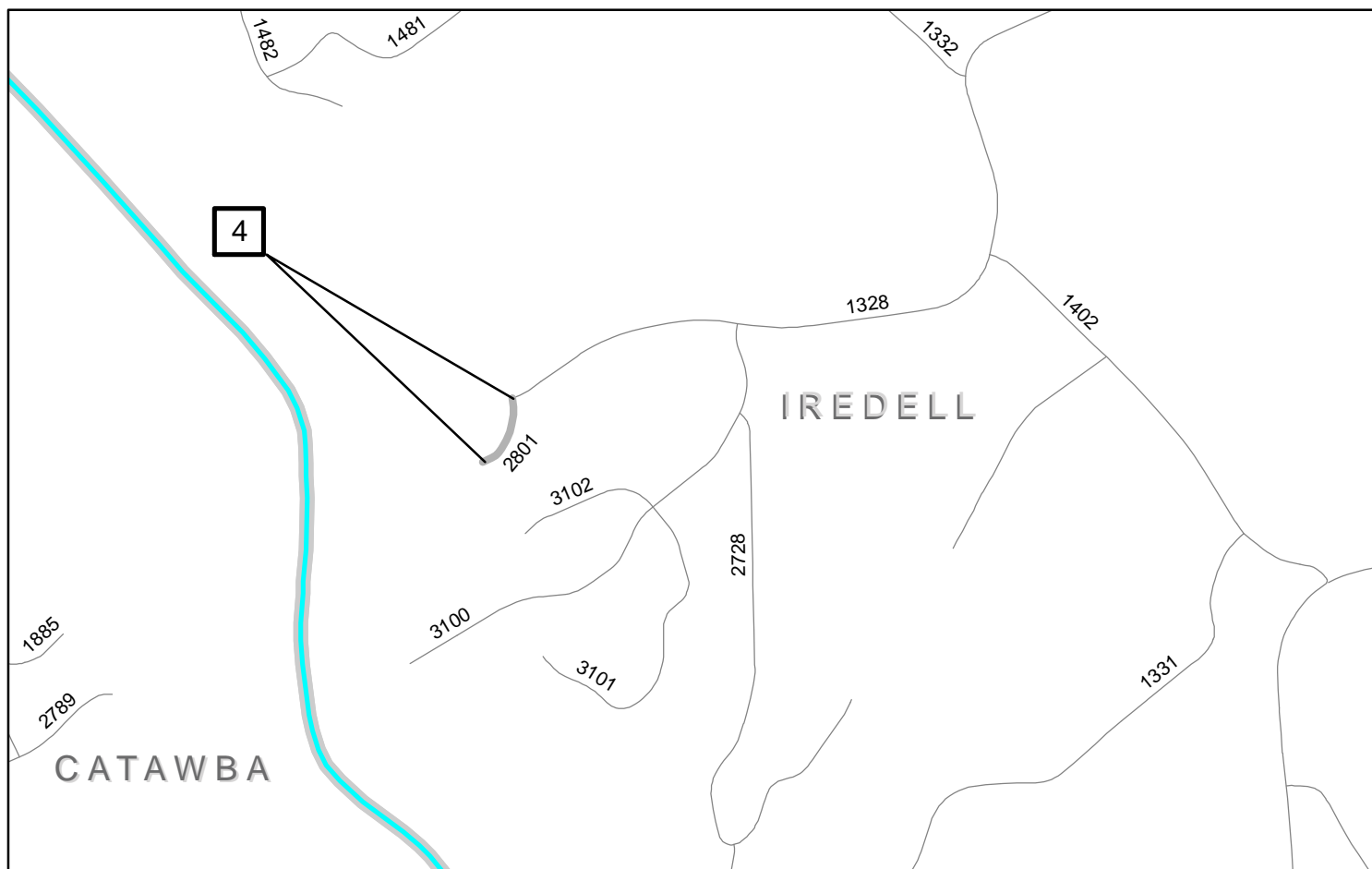
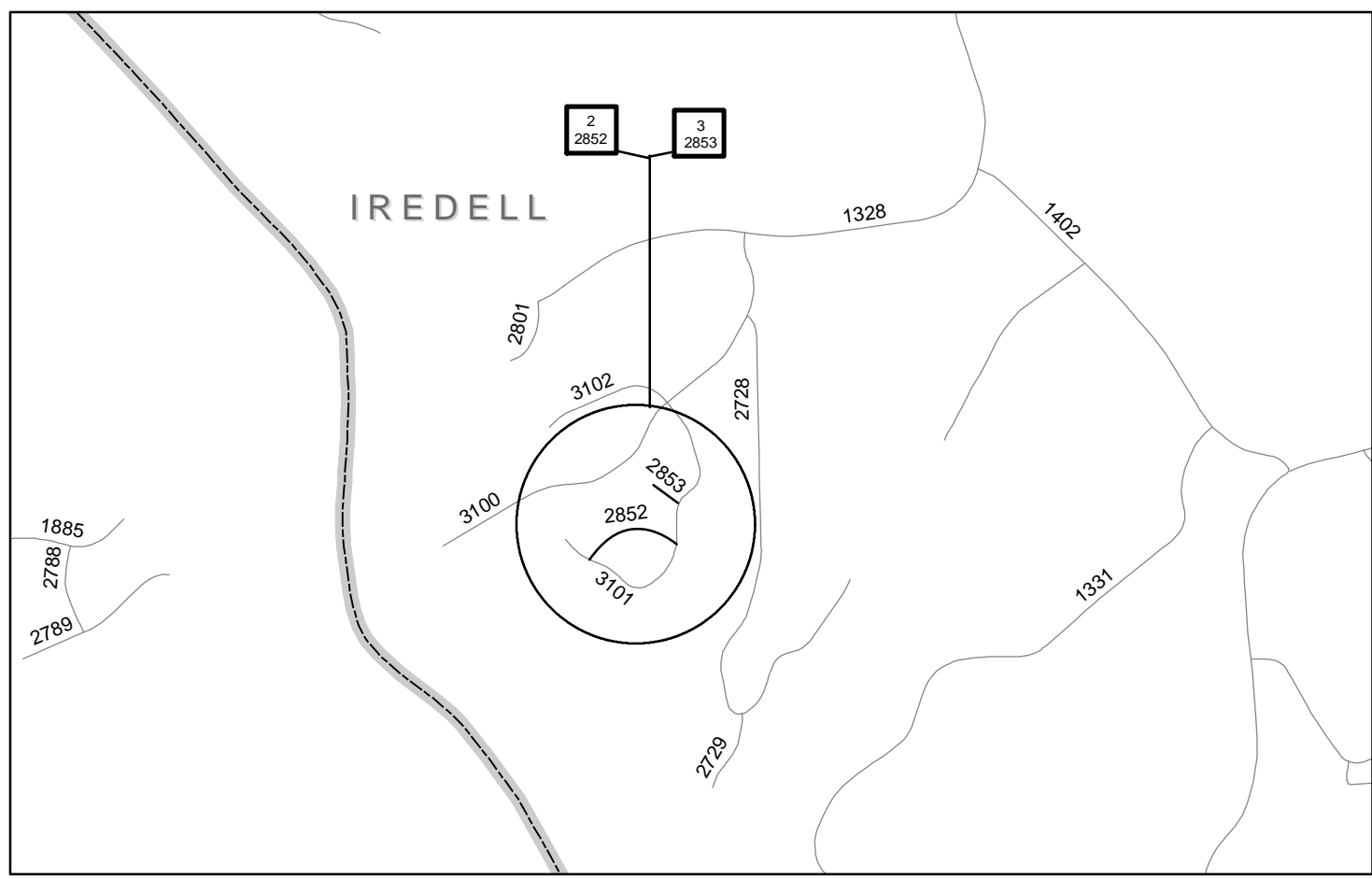
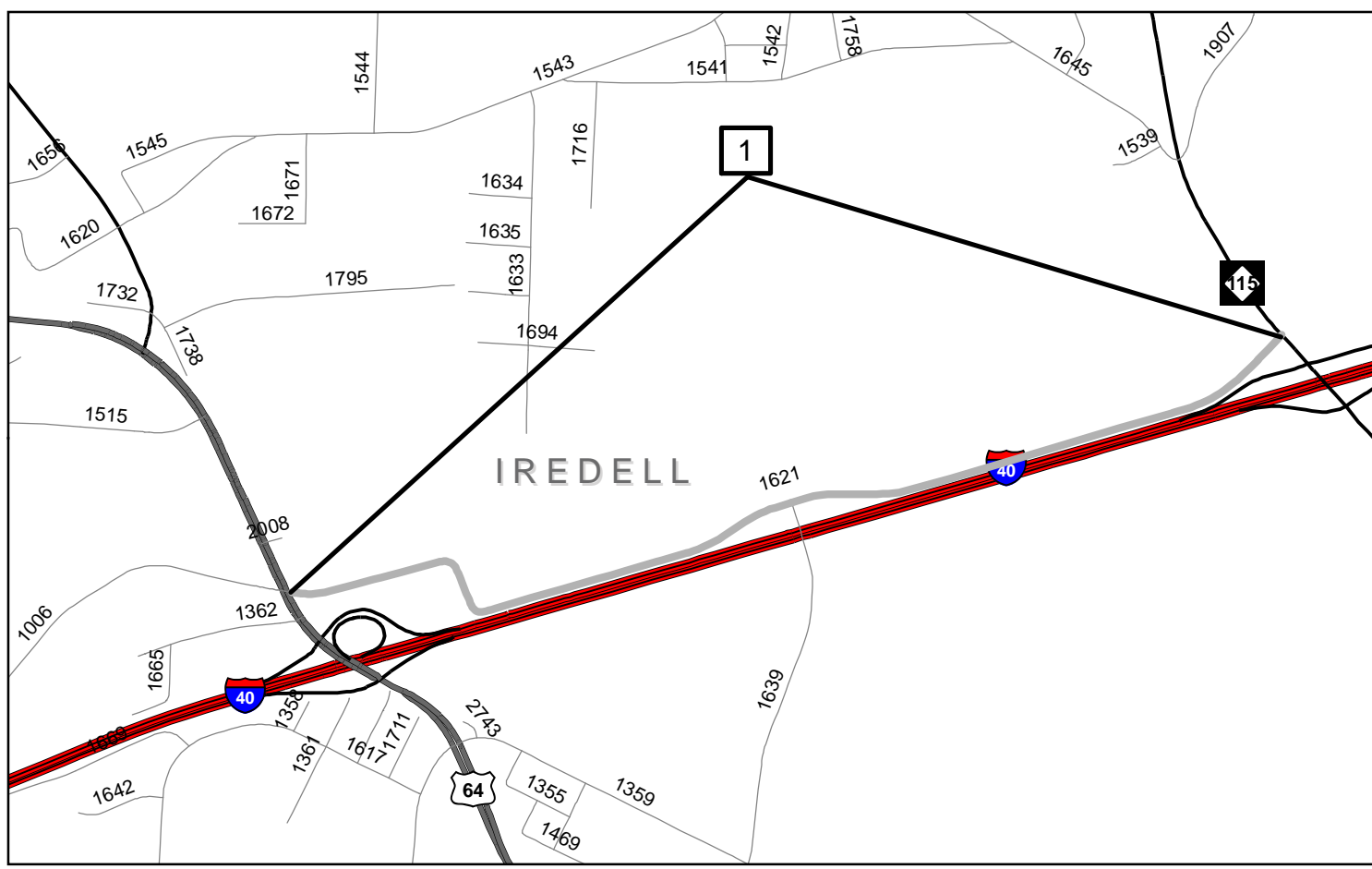
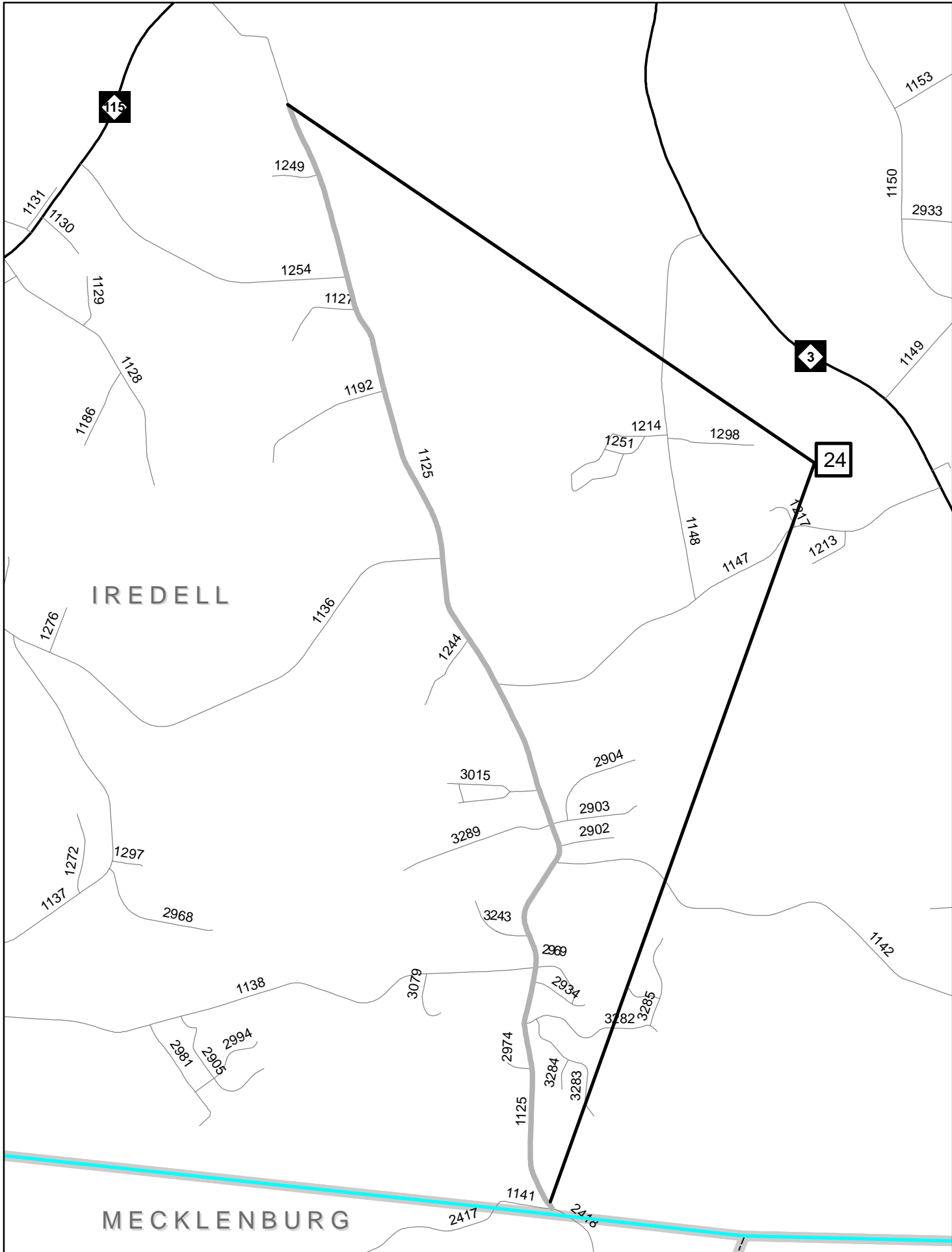


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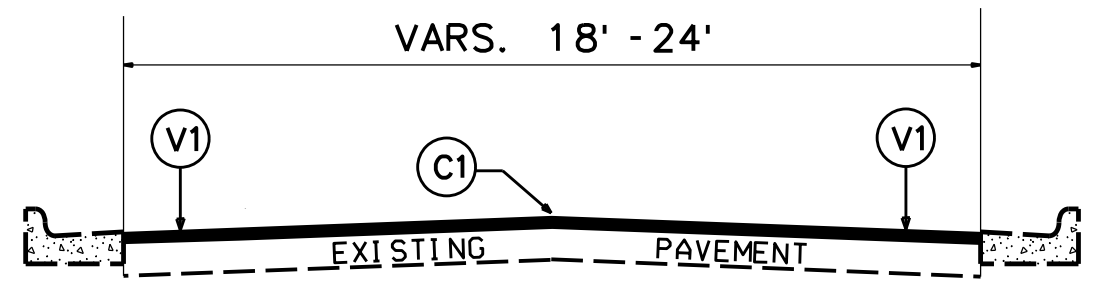
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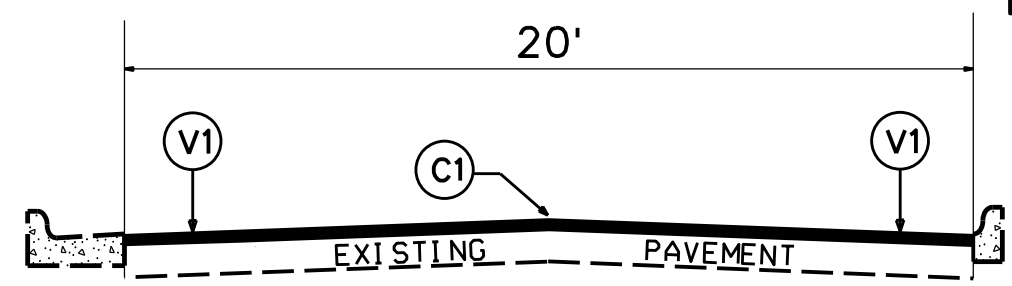


PROJ. REFERENCE NO.	SHEET NO.	TOTAL SHEETS
IREDELL COUNTY	6	
STATE PROJ. NO.	F. A. PROJ. NO.	DESCRIPTION
2026CPT. 12.17.20491		SECONDARY RESURFACING



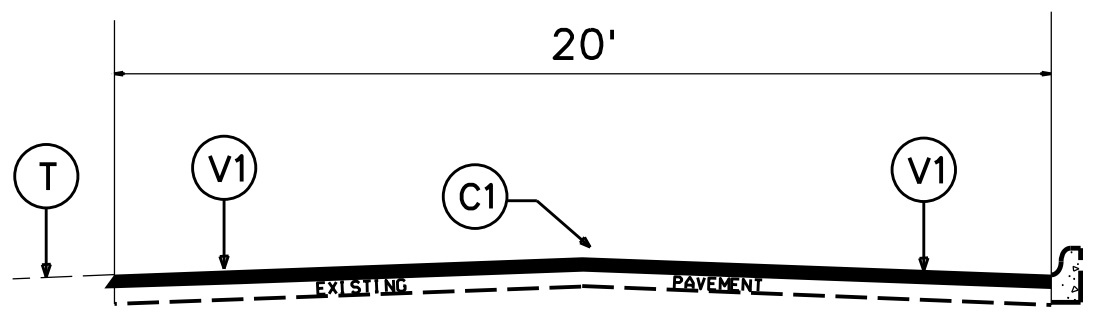
TYPICAL SECTION NO. 1

Map 1: 98+73 - 102+43
 Maps : 2, 3, 4, 12, 13, 14, 15, 16, 17, 18, 19, 20, 21 (ALL)



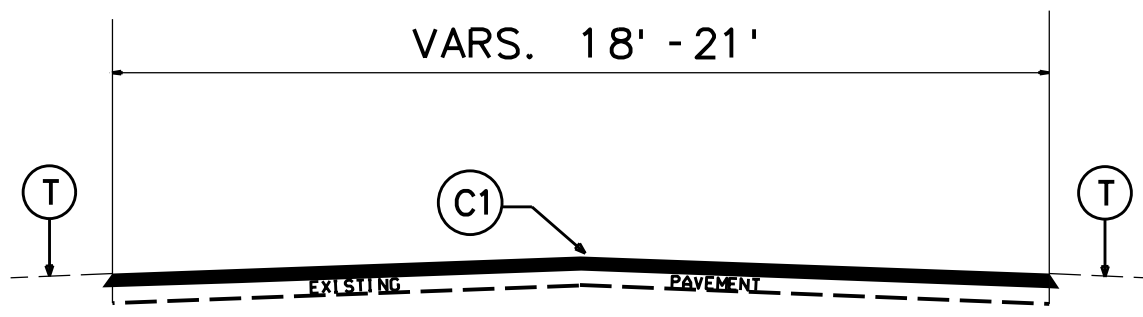
TYPICAL SECTION NO. 2

Map 1: 0+00 - 1+90



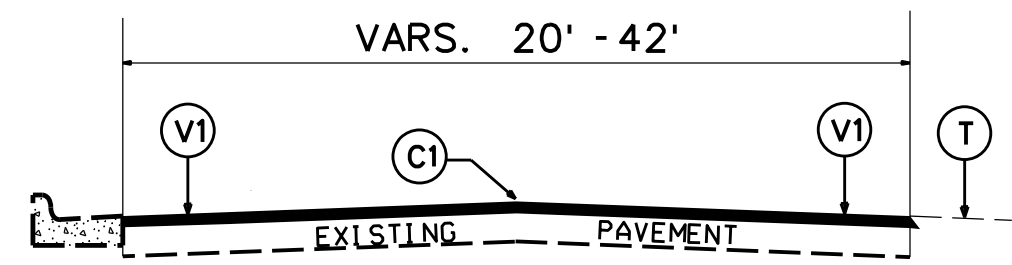
TYPICAL SECTION NO. 3

Map 1: 1+90 - 7+50



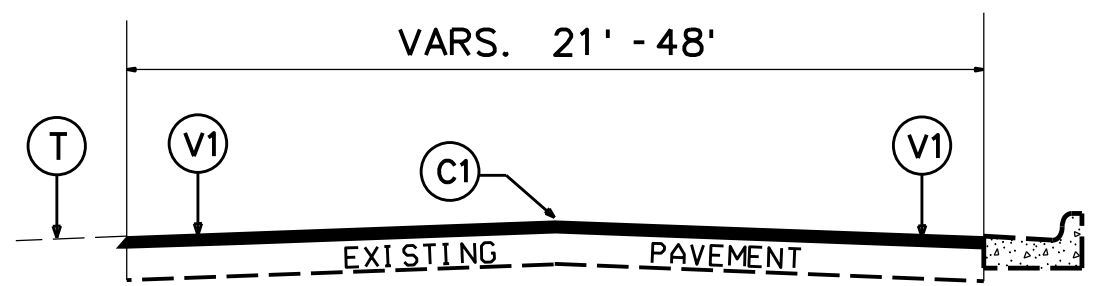
TYPICAL SECTION NO. 4

Map 1: 7+50 - 98+73
 Map 24: 0+00 - 75+50
 77+08 - 108+24
 110+88 - 139+92
 148+36 - 253+08
 Maps: 5, 6, 7, 8, 9, 10, 11, 22, 23 (ALL)



TYPICAL SECTION NO. 5

Map 1: 75+50 - 77+08
 Map 24: 139+92 - 148+36



TYPICAL SECTION NO. 6

Map 24: 108+24 - 110+88

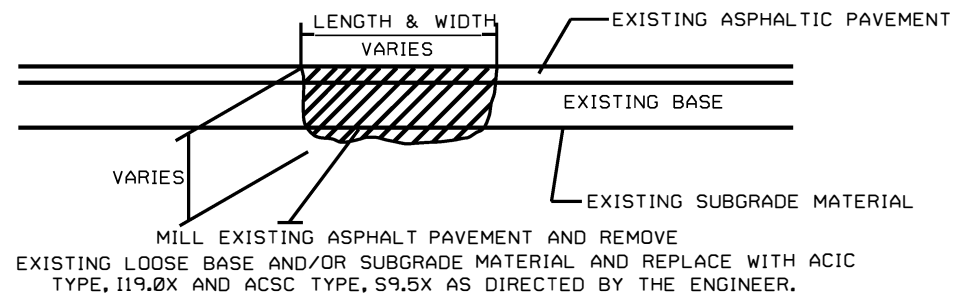
PAVEMENT SCHEDULE	
C1	PROP. APPROX. 1.5' ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5C, AT AN AVERAGE RATE OF 168 LBS. PER SQ. YD.
T	AGGREGATE SHOULDER BORROW (SHOULDER RECONSTRUCTION)
V1	MILL EXISTING ASPHALT PAVEMENT APPROX. 1.5' IN DEPTH

* ADDITIONAL MILLING WILL BE REQUIRED TO REMOVE EXISTING ASPHALT FROM GUTTER PAN

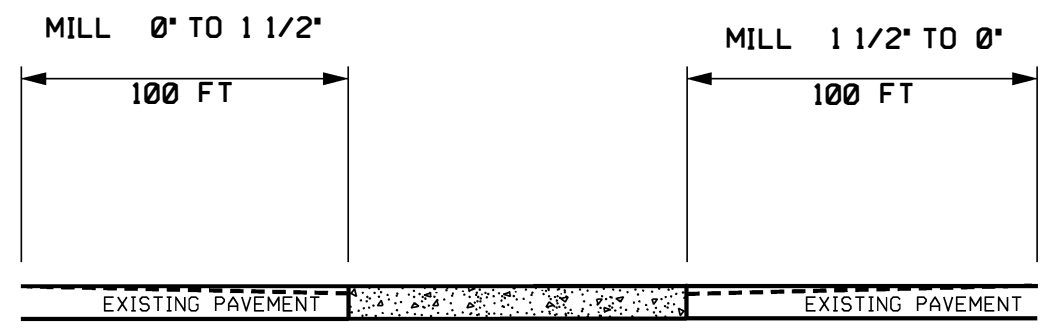
2026
 Resurfacing Program
 Typical Sections
 Iredell County

PROJ. REFERENCE NO.	SHEET NO.	TOTAL SHEETS
IREDELL COUNTY	7	
STATE PROJ. NO.	F.A. PROJ. NO.	DESCRIPTION
2026CPT. 12.17.20491		SECONDARY RESURFACING

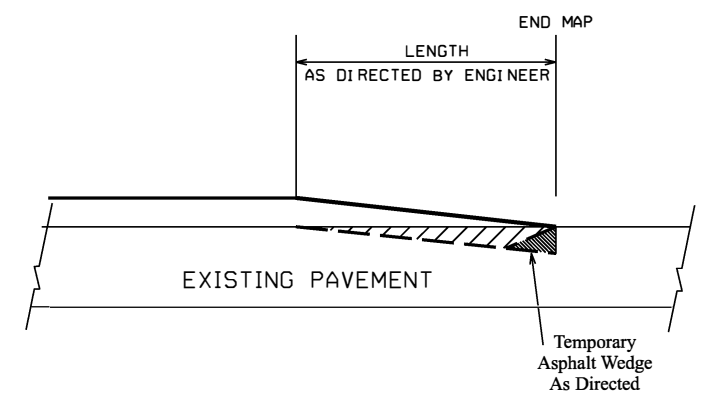
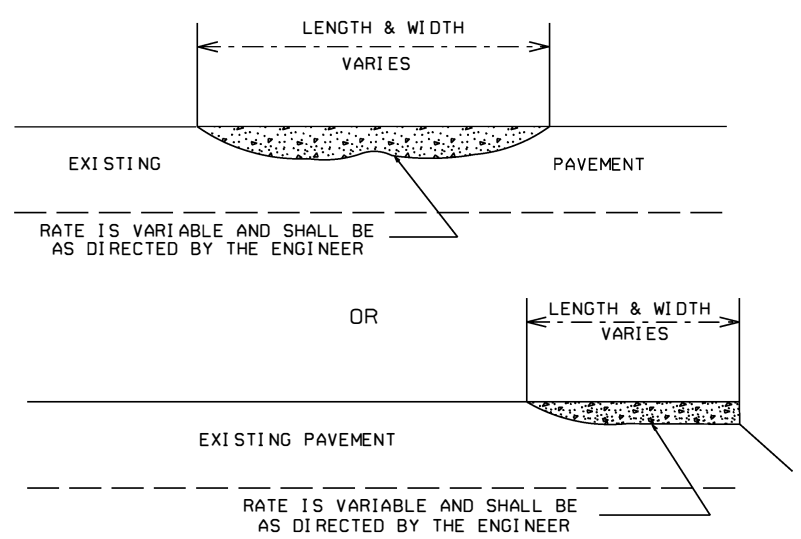
**DETAIL A
PATCHING EXISTING PAVEMENT**



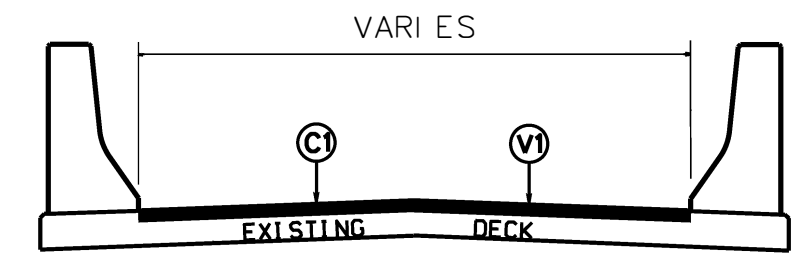
**DETAIL C
MILLING BRIDGE APPROACHES**



**DETAIL B
ASPHALT CONCRETE SURFACE COURSE
TYPE S9.5B & S9.5C (LEVELING COURSE)**



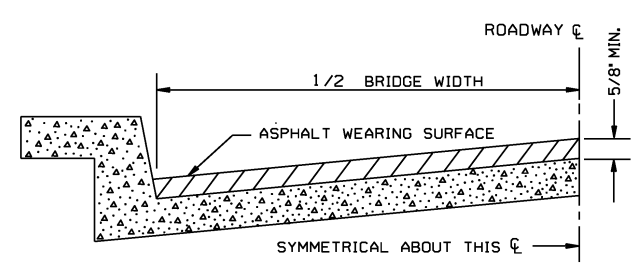
TIE-IN (INCIDENTAL) MILLING DETAIL



ASPHALT BRIDGE SECTION

** Use for all bridges with asphalt decks unless otherwise directed.

**DETAIL E
BRIDGE HALF TYPICAL SECTION**



FOR BRIDGES WITH FLOOR DRAINS, CARE SHALL BE EXERCISED IN PLACING THE WEARING SURFACE AROUND FLOOR DRAINS SO AS NOT TO HINDER EFFECTIVE DRAINAGE. ALL DRAINS SHALL BE LEFT OPEN.

THE PROPOSED WEARING SURFACE SHALL VARY IN THICKNESS AS NECESSARY TO PROVIDE A SMOOTH RIDING SURFACE. A THICKNESS OF NOT LESS THAN 1.5" SHALL BE PROVIDED. THE MAXIMUM THICKNESS SHALL PREFERABLY BE 1.5" UNLESS IT IS IMPRACTICAL TO PROVIDE A SMOOTH RIDING SURFACE OTHERWISE.

NOTES

BRIDGES TO BE RESURFACED AT LOCATIONS AND TO DEPTH AS DIRECTED BY THE ENGINEER.

PAVEMENT SCHEDULE	
C1	PROP. APPROX. 1.5" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5C. AT AN AVERAGE RATE OF 168 LBS. PER SQ. YD.
T	AGGREGATE SHOULDER BORROW (SHOULDER RECONSTRUCTION)
V1	MILL EXISTING ASPHALT PAVEMENT APPROX. 1.5" IN DEPTH

* ADDITIONAL MILLING WILL BE REQUIRED TO REMOVE EXISTING ASPHALT FROM GUTTER PAN

PROJECT NO.	SHEET NO.	TOTAL NO.
2026CPT.12.17.20491		

SUMMARY OF QUANTITIES

PROJECT NO	COUNTY	MAP NO	ROUTE	DESCRIPTION	TYP NO	LANES	LANE TYPE	LENGTH	WIDTH	BEG N	END MP	0255000000-E	1220000000-E	1245000000-E	1297000000-E	1308000000-E	1330000000-E	1523000000-E	1524000000-E	1575000000-E	1704000000-E	2815000000-N	2830000000-N	2845000000-N	7324000000-E	7440000000-E	7456100000-E					
								MI	FT			AGGREGATE BORROW	INCIDENTAL STONE BASE	SHOULDER RECONSTRUCTION	1 1/2" MILLING	0" TO 1.5" MILLING	INCIDENTAL MILLING	SURFACE COURSE, S9.5C	LEVELING COURSE, S9.5C	ASPHALT BINDER FOR PLANT MIX	PATCHING EXISTING PAVEMENT	ADJ. OF DROP INLET	ADJ. OF MANHOLES	ADJ. OF METER OR VALVE BOX	JUNCTION BOX (STD.)	INDUCTIVE LOOP	LEAD-IN CABLE (14-2)					
												TON	TONS	SMI	SY	SY	SY	TONS	TONS	TONS	TONS	EA	EA	EA	EA	LF	LF					
2026CPT.12.17.20491	Iredell	1	SR-1621 / NORTHSIDE DR	FROM NC 115 (N. CENTER STREET) TO US 64 (TALYORSVILLE HWY)	1,2,3,4,5	2		1.942	20	0	1.942	641	75	3.66	1,210	125	100	2,105	85	150	110	3	7	2	1	624	100					
TOTAL FOR MAP NO. 1												641	75	3.66	1,210	125	100	2,105	85	150	110	3	7	2	1	624	100					
2026CPT.12.17.20491	Iredell	2	SR-2852 / HABERSHAM LOOP	FROM SR 3101 (E. TATTERSALL DR.) TO SR 3101 (E. TATTERSALL DR.)	1	2		0.136	24	0	0.136				1,930			178		11												
TOTAL FOR MAP NO. 2															1,930			178		11												
2026CPT.12.17.20491	Iredell	3	SR-2853 / CROFTON CT	FROM SR 3101 (E. TATTERSALL DR.) TO CUL-DE-SAC	1	2		0.034	24	0	0.034				480			45		3												
TOTAL FOR MAP NO. 3															480			45		3												
2026CPT.12.17.20491	Iredell	4	SR-2801 / HIGHLAND VIEW DR	FROM SR 1328 (E. MOMBO) TO CUL-DE-SAC	1	2		0.093	18	0	0.093				982			90		5												
TOTAL FOR MAP NO. 4															982			90		5												
2026CPT.12.17.20491	Iredell	5	SR-1331 / CLARK COVE RD	FROM SR 1402 (ST. JOHNS RD.) TO END OF MAINTENANCE	4	2		0.948	18	0	0.948	313	20	1.90			30	925	100	77	350											
TOTAL FOR MAP NO. 5												313	20	1.90			30	925	100	77	350											
2026CPT.12.17.20491	Iredell	6	SR-1449 / RIVERBEND DR	FROM SR 1300 (FERN HILL RD.) TO END OF MAINTENANCE	4	2		0.64	20	0	0.64	211	15	1.28			30	695	130	51	55											
TOTAL FOR MAP NO. 6												211	15	1.28			30	695	130	51	55											
2026CPT.12.17.20491	Iredell	7	SR-1332 / PINEVILLE RD	FROM SR 1004 (BUFFALO SHOALS RD.) TO SR 1328 (E. MONBO RD.)	4	2		2.462	18	0	2.462	812	50	4.92			100	2,410	250	159	40											
TOTAL FOR MAP NO. 7												812	50	4.92			100	2,410	250	159	40											
2026CPT.12.17.20491	Iredell	8	SR-2150 / STROUD RD	FROM SR 2145 (VAUGHN MILL RD.) TO END OF MAINTENANCE	4	2		0.646	18	0	0.646	213	15	1.29			35	630	100	49	125											
TOTAL FOR MAP NO. 8												213	15	1.29			35	630	100	49	125											
2026CPT.12.17.20491	Iredell	9	SR-2483 / ROSELAND CT	FROM SR 1389 (SHEPHERDS RD.) TO END OF MAINTENANCE	4	2		0.24	18	0	0.244	63	10	0.48			20	225	100	20	75											
TOTAL FOR MAP NO. 9												63	10	0.48			20	225	100	20	75											
2026CPT.12.17.20491	Iredell	10	SR-2484 / WILD ROSE LOOP	FROM SR 2483 (ROSELAND CT.) TO SR 2483 (ROSELAND CT.)	4	2		0.069	18	0	0.069	23	5	0.14				67	15	6	15											
TOTAL FOR MAP NO. 10												23	5	0.14				67	15	6	15											
2026CPT.12.17.20491	Iredell	11	SR-2554 / SHEPHERD VALLEY RD	FROM SR 1389 (SHEPHERDS RD.) TO END OF MAINTENANCE	4	2		0.224	18	0	0.224	81	5	0.44			20	220	15	15	30											
TOTAL FOR MAP NO. 11												81	5	0.44			20	220	15	15	30											
2026CPT.12.17.20491	Iredell	12	SR-3097 / TEMPLETON BAY DR	FROM SR 1108 (TEMPLETON RD.) TO CUL-DE-SAC	1	2		0.438	20	0	0.438				5,140			500		28												
TOTAL FOR MAP NO. 12															5,140			500		28												
2026CPT.12.17.20491	Iredell	13	SR-3099 / PAULS LN	FROM SR 3097 (TEMPLETON BAY) TO CUL-DE-SAC	1	2		0.114	20	0	0.114				1,340			150		7												
TOTAL FOR MAP NO. 13															1,340			150		7												
2026CPT.12.17.20491	Iredell	14	SR-3098 / PARKSIDE LN	FROM SR 3186 (HUNTFIELD WAY) TO SR 3097 (TEMPLETON BAY)	1	2		0.235	20	0	0.235				2,760			255		15												
TOTAL FOR MAP NO. 14															2,760			255		15												
2026CPT.12.17.20491	Iredell	15	SR-3186 / HUNTFIELD WAY	FROM SR 3097 (TEMPLETON BAY) TO CUL-DE-SAC	1	2		0.268	20	0	0.268				3,150			305		17												
TOTAL FOR MAP NO. 15															3,150			305		17												
2026CPT.12.17.20491	Iredell	16	SR-3061 / POLPIS DR	FROM SR 1100 (BRAWLEY SCHOOL RD.) TO CUL-DE-SAC	1	2		0.288	24	0	0.288				4,060			385		22												
TOTAL FOR MAP NO. 16															4,060			385		22												
2026CPT.12.17.20491	Iredell	17	SR-3062 / JEREMY POINT PL	FROM SR 3061 (POPIS RD.) TO SR 3063 (BRICK KILN WAY)	1	2		0.039	24	0	0.039				565			52		3												
TOTAL FOR MAP NO. 17															565			52		3												
2026CPT.12.17.20491	Iredell	18	SR-3063 / BRICK KILN WAY	FROM SR 3062 (JEREMY POINT) TO CUL-DE-SAC	1	2		0.251	24	0	0.251				3,535			330		19												
TOTAL FOR MAP NO. 18															3,535			330		19												
2026CPT.12.17.20491	Iredell	19	SR-3292 / MASTHEAD CT	FROM SR 3063 (BRICK KILN WAY) TO CUL-DE-SAC	1	2		0.029	24	0	0.029				650			60		4												
TOTAL FOR MAP NO. 19															650			60		4												
2026CPT.12.17.20491	Iredell	20	SR-3064 / UNION CHAPEL DR	FROM SR 3063 (BRICK KILN WAY) TO CUL-DE-SAC	1	2		0.224	24	0	0.224				3,155			291		17												
TOTAL FOR MAP NO. 20															3,155			291		17												
2026CPT.12.17.20491	Iredell	21	SR-3256 / WESCOE CT	FROM SR 3064 (UNION CHAPEL DR.) TO CUL-DE-SAC	1	2		0.031	24	0	0.031				675			65		4												
TOTAL FOR MAP NO. 21															675			65		4												
2026CPT.12.17.20491	Iredell	22	SR-2315 / LONE PINE RD	FROM US 64 (MOCKSVILLE HWY) TO END OF MAINTENANCE	4	2		0.76	18	0	0.76	251	5	1.52				745	100	59	200											
TOTAL FOR MAP NO. 22												251	5	1.52				745	100	59	200											
2026CPT.12.17.20491	Iredell	23	SR-1887 / TATUM RD	FROM SR 1891 (OLIN LOOP) TO SR 1845 (TABOR RD.)	4	2		2.198	20	0	2.198	725	20	4.40			80	2,385	150	184	300											
TOTAL FOR MAP NO. 23												725	20	4.40			80	2,385	150	184	300											
2026CPT.12.17.20491	Iredell	24	SR-1125 / SHEARERS RD	FROM NEW PAVEMENT AT BRAWLEY AVE TO COUNTY LINE	4,5,6	2		4.795	VAR. 21-48	0	4.795	1,582	75	9.40	5,000		200	5,800	250	400	200		3		1	468	75					
TOTAL FOR MAP NO. 24												1,582	75	9.40	5,000		200	5,800	250	400	200		3		1	468	75					
TOTAL FOR PROJ NO. 2026CPT.12.17.20491												17,104			4,915	295	29.43	34,632		125		615	18,913	1,295	1,325	1,500	3	10	2	2	1,092	175
GRAND TOTAL												17,104			4,915	295	29.43	34,632		125		615	18,913	1,295	1,325	1,500	3	10	2	2	1,092	175

PROJECT NO.	SHEET NO.	TOTAL NO.
2026CPT.12.17.20491		

THERMOPLASTIC AND PAINT QUANTITIES

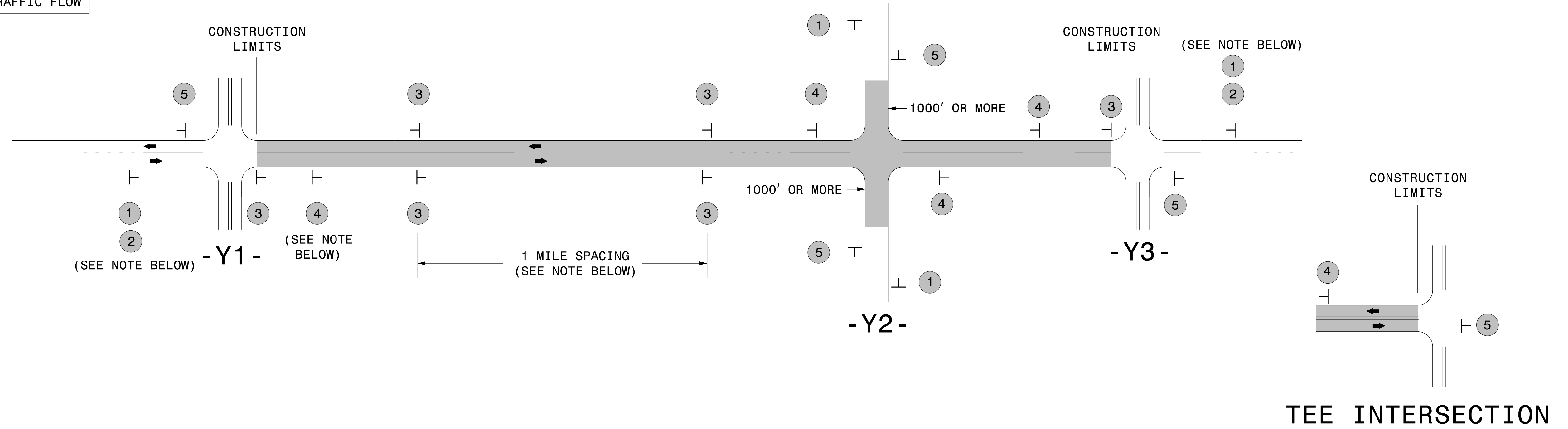
PROJECT NO	COUNTY	MAP NO	ROUTE	DESCRIPTION	TYP NO	LANES	LANE TYPE	LENGTH	WIDTH	BEGIN MP	END MP	4413000000-N	4457000000-N	4510000000-E	4685000000-E		4695000000-E	4700000000-E	4704000000-E	4709000000-E	4720000000-E	4725000000-E				4810000000-E		4892000000-N									
												WORK ZONE ADV / GEN WARNING SIGNS	TEMPORARY TRAFFIC CONTROL	LAW ENFORCEMENT	THERMO PAVEMENT MARKING LINES, YELLOW (4", 90 MILS)	THERMO PAVEMENT MARKING LINES, WHITE (4", 90 MILS)	THERMO PAVEMENT MARKING LINES, WHITE (8", 90 MILS)	THERMO PAVEMENT MARKING LINES, YELLOW (12", 90 MILS)	THERMO PAVEMENT MARKING LINES, WHITE (16", 90 MILS)	THERMO PAVEMENT MARKING LINES, WHITE (24", 90 MILS)	THERMO RXR MSG 90M	THERMO LT ARROW 90 M	THERMO RT ARROW 90 M	THERMO STR & LT ARROW 90M	THERMO STR & RT ARROW 90 M	4" WHITE PAINT	4" YELLOW PAINT	H-SHAPED POLYCARBONATE PAVEMENT MARKERS									
												MI	FT	SF	LS	HR	LF	LF	LF	LF	EA	EA	EA	EA	EA	EA	EA	EA									
2026CPT.12.17.20491	Iredell	1	SR-1621 / NORTHSIDE DR	FROM NC 115 (N. CENTER STREET) TO US 64 (TALYORSVILLE HWY)	1,2,3,4,5	2		1.942	20	0	1.942	134	*	5	22,560	22,560			80	100	2	1	1	1	1												
TOTAL FOR MAP NO. 1												1.942		134		5	22,560	22,560			80	100	2	1	1	1	1										
2026CPT.12.17.20491	Iredell	2	SR-2852 / HABERSHAM LOOP	FROM SR 3101 (E. TATTERSALL DR.) TO SR 3101 (E. TATTERSALL DR.)	1	2		0.136	24	0	0.136	134	*																								
TOTAL FOR MAP NO. 2												0.136		134																							
2026CPT.12.17.20491	Iredell	3	SR-2853 / CROFTON CT	FROM SR 3101 (E. TATTERSALL DR.) TO CUL-DE-SAC	1	2		0.034	24	0	0.034	51	*																								
TOTAL FOR MAP NO. 3												0.034		51																							
2026CPT.12.17.20491	Iredell	4	SR-2801 / HIGHLAND VIEW DR	FROM SR 1328 (E. MOMBO) TO CUL-DE-SAC	1	2		0.093	18	0	0.093	51	*																								
TOTAL FOR MAP NO. 4												0.093		51																							
2026CPT.12.17.20491	Iredell	5	SR-1331 / CLARK COVE RD	FROM SR 1402 (ST. JOHNS RD.) TO END OF MAINTENANCE	4	2		0.948	18	0	0.948	51	*														20,021	20,021									
TOTAL FOR MAP NO. 5												0.948		51																			20,021	20,021			
2026CPT.12.17.20491	Iredell	6	SR-1449 / RIVERBEND DR	FROM SR 1300 (FERN HILL RD.) TO END OF MAINTENANCE	4	2		0.64	20	0	0.64	51	*														13,520	13,520									
TOTAL FOR MAP NO. 6												0.64		51																				13,520	13,520		
2026CPT.12.17.20491	Iredell	7	SR-1332 / PINEVILLE RD	FROM SR 1004 (BUFFALO SHOALS RD.) TO SR 1328 (E. MONBO RD.)	4	2		2.462	18	0	2.462	134	*														52,000	52,000									
TOTAL FOR MAP NO. 7												2.462		134																				52,000	52,000		
2026CPT.12.17.20491	Iredell	8	SR-2150 / STROUD RD	FROM SR 2145 (VAUGHN MILL RD.) TO END OF MAINTENANCE	4	2		0.646	18	0	0.646	51	*														13,650	13,650									
TOTAL FOR MAP NO. 8												0.646		51																				13,650	13,650		
2026CPT.12.17.20491	Iredell	9	SR-2483 / ROSELAND CT	FROM SR 1389 (SHEPHERDS RD.) TO END OF MAINTENANCE	4	2		0.24	18	0	0.244	51	*																								
TOTAL FOR MAP NO. 9												0.24		51																							
2026CPT.12.17.20491	Iredell	10	SR-2484 / WILD ROSE LOOP	FROM SR 2483 (ROSELAND CT.) TO SR 2483 (ROSELAND CT.)	4	2		0.069	18	0	0.069	51	*																								
TOTAL FOR MAP NO. 10												0.069		51																							
2026CPT.12.17.20491	Iredell	11	SR-2554 / SHEPHERD VALLEY RD	FROM SR 1389 (SHEPHERDS RD.) TO END OF MAINTENANCE	4	2		0.224	18	0	0.224	51	*																								
TOTAL FOR MAP NO. 11												0.224		51																							
2026CPT.12.17.20491	Iredell	12	SR-3097 / TEMPLETON BAY DR	FROM SR 1108 (TEMPLETON RD.) TO CUL-DE-SAC	1	2		0.438	20	0	0.438	51	*																								
TOTAL FOR MAP NO. 12												0.438		51																							
2026CPT.12.17.20491	Iredell	13	SR-3099 / PAULS LN	FROM SR 3097 (TEMPLETON BAY) TO CUL-DE-SAC	1	2		0.114	20	0	0.114	51	*																								
TOTAL FOR MAP NO. 13												0.114		51																							
2026CPT.12.17.20491	Iredell	14	SR-3098 / PARKSIDE LN	FROM SR 3186 (HUNTFIELD WAY) TO SR 3097 (TEMPLETON BAY)	1	2		0.235	20	0	0.235	134	*																								
TOTAL FOR MAP NO. 14												0.235		134																							
2026CPT.12.17.20491	Iredell	15	SR-3186 / HUNTFIELD WAY	FROM SR 3097 (TEMPLETON BAY) TO CUL-DE-SAC	1	2		0.268	20	0	0.268	51	*																								
TOTAL FOR MAP NO. 15												0.268		51																							
2026CPT.12.17.20491	Iredell	16	SR-3061 / POLPIS DR	FROM SR 1100 (BRAWLEY SCHOOL RD.) TO CUL-DE-SAC	1	2		0.288	24	0	0.288	51	*				160											72									
TOTAL FOR MAP NO. 16												0.288		51					160																72		
2026CPT.12.17.20491	Iredell	17	SR-3062 / JEREMY POINT PL	FROM SR 3061 (POPIS RD.) TO SR 3063 (BRICK KILN WAY)	1	2		0.039	24	0	0.039	134	*																								
TOTAL FOR MAP NO. 17												0.039		134																							
2026CPT.12.17.20491	Iredell	18	SR-3063 / BRICK KILN WAY	FROM SR 3062 (JEREMY POINT) TO CUL-DE-SAC	1	2		0.251	24	0	0.251	51	*																								
TOTAL FOR MAP NO. 18												0.251		51																							
2026CPT.12.17.20491	Iredell	19	SR-3292 / MASTHEAD CT	FROM SR 3063 (BRICK KILN WAY) TO CUL-DE-SAC	1	2		0.029	24	0	0.029	51	*																								
TOTAL FOR MAP NO. 19												0.029		51																							
2026CPT.12.17.20491	Iredell	20	SR-3064 / UNION CHAPEL DR	FROM SR 3063 (BRICK KILN WAY) TO CUL-DE-SAC	1	2		0.224	24	0	0.224	51	*																								
TOTAL FOR MAP NO. 20												0.224		51																							
2026CPT.12.17.20491	Iredell	21	SR-3256 / WESCOE CT	FROM SR 3064 (UNION CHAPEL DR.) TO CUL-DE-SAC	1	2		0.031	24	0	0.031	51	*																								
TOTAL FOR MAP NO. 21												0.031		51																							
2026CPT.12.17.20491	Iredell	22	SR-2315 / LONE PINE RD	FROM US 64 (MOCKVILLE HWY) TO END OF MAINTENANCE	4	2		0.76	18	0	0.76	67	*																								
TOTAL FOR MAP NO. 22												0.76		67																							
2026CPT.12.17.20491	Iredell	23	SR-1887 / TATUM RD	FROM SR 1891 (OLIN LOOP) TO SR 1845 (TABOR RD.)	4	2		2.198	20	0	2.198	166	*														46,425										
TOTAL FOR MAP NO. 23												2.198		166																			46,425	46,425			
2026CPT.12.17.20491	Iredell	24	SR-1125 / SHEARERS RD	FROM NEW PAVEMENT AT BRAWLEY AVE TO COUNTY LINE	4,5,6	2		4.795	VARS. 21-48	0	4.795	194	*	10	55,700	55,700	48	300		60		12	7														
TOTAL FOR MAP NO. 24												4.795		194		10	55,700	55,700	48	300		12	7													375	
TOTAL FOR PROJ NO. 2026CPT.12.17.20491												17.104		1,913		1			15	78,260	78,260	208	300	80	160	2	13	9	24	1	1			145,616	145,688	375	
																		156,520															291,304				

SIGNING FOR RESURFACING PROJECTS

LEGEND

┆ STATIONARY SIGN

← DIRECTION OF TRAFFIC FLOW



MAINLINE (-L-) SIGNING

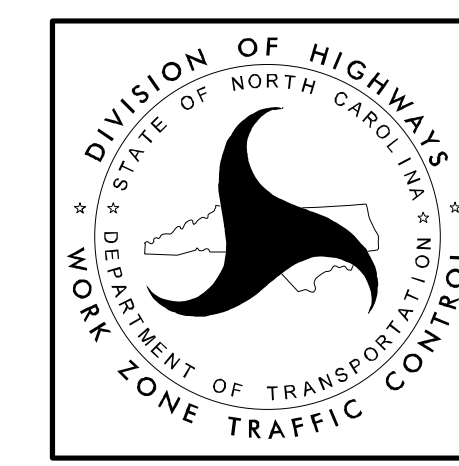
-Y- LINE SIGNING

SIGNING NOTES AND PLACEMENT PER DIRECTION	1		PLACE 1000' PRIOR TO BEGINNING OF CONSTRUCTION LIMITS. ONLY USED ON -Y- LINES IF RESURFACING LIMITS EXTEND 1000' ALONG -Y- LINE.	<p>NO REQUIRED STATIONARY SIGNING FOR THE FOLLOWING -Y- LINE CONDITIONS:</p> <ol style="list-style-type: none"> LESS THAN 1000' OF RESURFACING ALONG -Y- LINE SUBDIVISION ROADS DEAD END ROADS <p>WHEN PAVING/CONSTRUCTION ACTIVITIES PROCEED ACROSS AN UNSIGNED -Y- LINE, PORTABLE ADVANCE WARNING SIGNS SHALL BE USED ALONG THE -Y- LINE AS SHOWN BELOW. REMOVE UPON COMPLETION OF WORK.</p> <div style="display: flex; justify-content: space-around;"> <div style="text-align: center;"> PLACED 500' IN ADVANCE OF FLAGGER. </div> <div style="text-align: center;"> PLACED 250' IN ADVANCE OF FLAGGER. </div> </div>
	2		#2 SIGN ONLY USED WHEN CONSTRUCTION LIMITS ARE 2 OR MORE MILES IN LENGTH. ROUND UP TO NEXT WHOLE NUMBER. (NO FRACTIONAL OR DECIMAL NUMBERS)	
	3		- PLACE INITIALLY AT THE CONSTRUCTION LIMITS AND SPACE 1 MILE APART THEREAFTER. - AT TEE INTERSECTIONS INSTALL INITIALLY 1/2 MILE FROM INTERSECTION AND SPACE 1 MILE APART THEREAFTER.	
	4		- THESE ARE FOR -Y- LINES THAT ARE "THROUGH" ROADWAYS. - DEAD END AND SUBDIVISION ROADS ARE NOT "THROUGH" ROADWAYS. - INSTALL 500' +/- FROM EACH -Y- LINE APPROACH AS SHOWN ABOVE. - FOR MULTIPLE -Y- LINES THAT ARE SEPARATED BY 0.25 MILES OR LESS, TREAT AS A SINGLE UNIT AND INSTALL WITHIN 500' OF EACH APPROACH. - A MAXIMUM OF 2 SIGN SETS PER MILE. DO NOT INSTALL WHEN -Y- LINES ARE WITHIN 0.5 MILES FROM "END ROAD WORK" SIGN. - FOR TEE INTERSECTIONS, INSTALL WITHIN 500' +/- OF THE INTERSECTION ALONG -L- LINE.	
	5		PLACE 500' FOLLOWING THE END OF CONSTRUCTION LIMITS OR AS SHOWN WHEN WORK ENDS AT A 3-WAY TEE INTERSECTION.	

THE ABOVE SIGNS ARE ALL THAT ARE REQUIRED FOR A CONTRACTOR TO BEGIN A RESURFACING CONTRACT. ANY ADDITIONAL SIGNS REQUESTED BY NCDOT DIVISIONS SHALL BE INSTALLED WITHIN 7 BUSINESS DAYS OF THE START OF CONTRACT WORK.

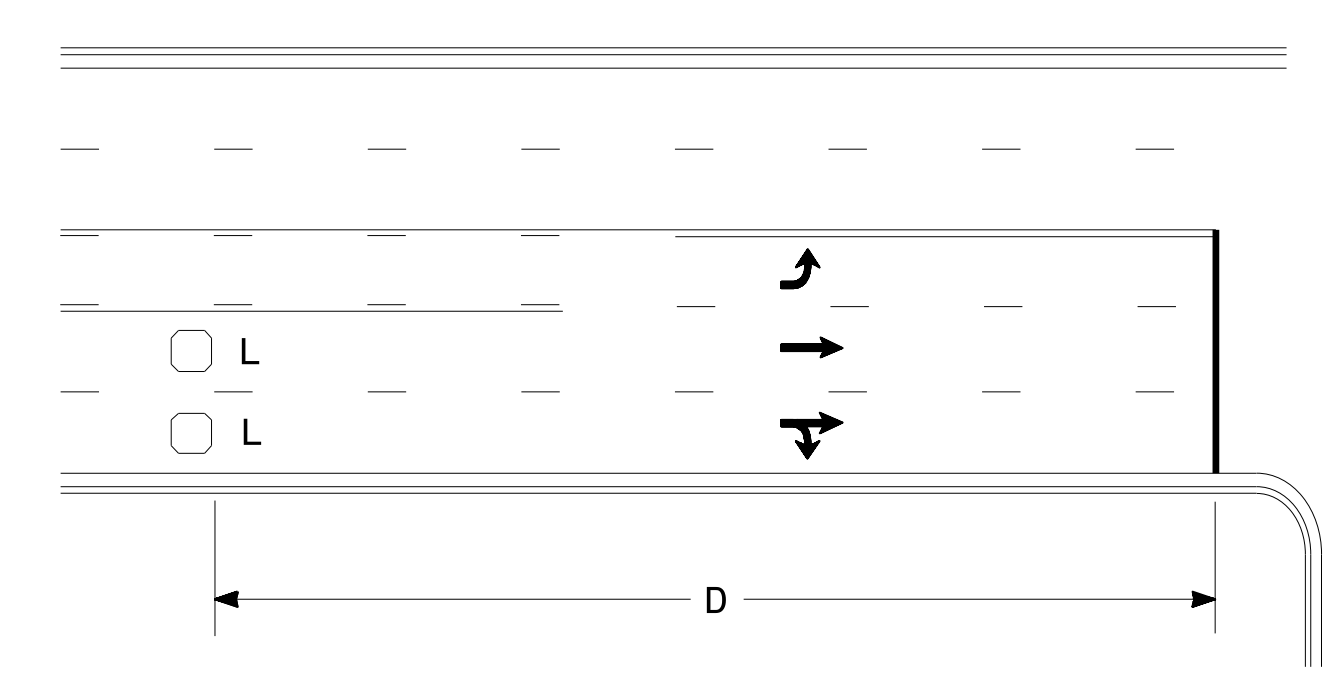
MAPS LESS THAN 2 MILES

FOR RESURFACING MAPS WITH CONSTRUCTION LIMITS LESS THAN 2 MILES IN LENGTH, NO STATIONARY SIGNS ARE REQUIRED. USE PORTABLE "ROAD UNDER CONSTRUCTION" OR "ROAD WORK AHEAD" SIGNS IN LIEU OF STATIONARY ADVANCE WARNINGS SIGNS.



ADVANCE WARNING SIGNS FOR RURAL AND SUBURBAN 2-LANE ROADWAY RESURFACING

High Speed Detection (≥40 mph)

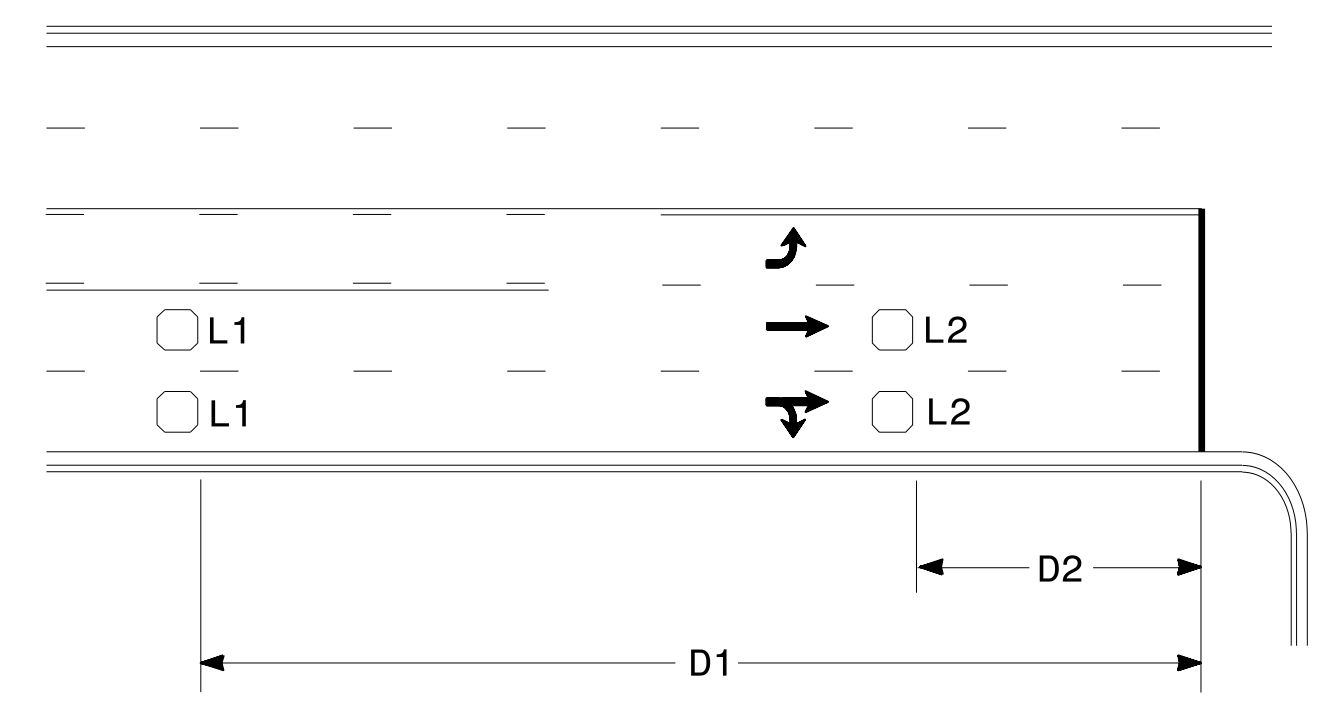


Speed Limit mph	D ft
40	250
45	300
50	355
55	420

L = 6ft X 6ft
Wired in series for TS1
Controllers
Wired separately for TS2,
170, and 2070L Controllers

Volume Density Operation

OR

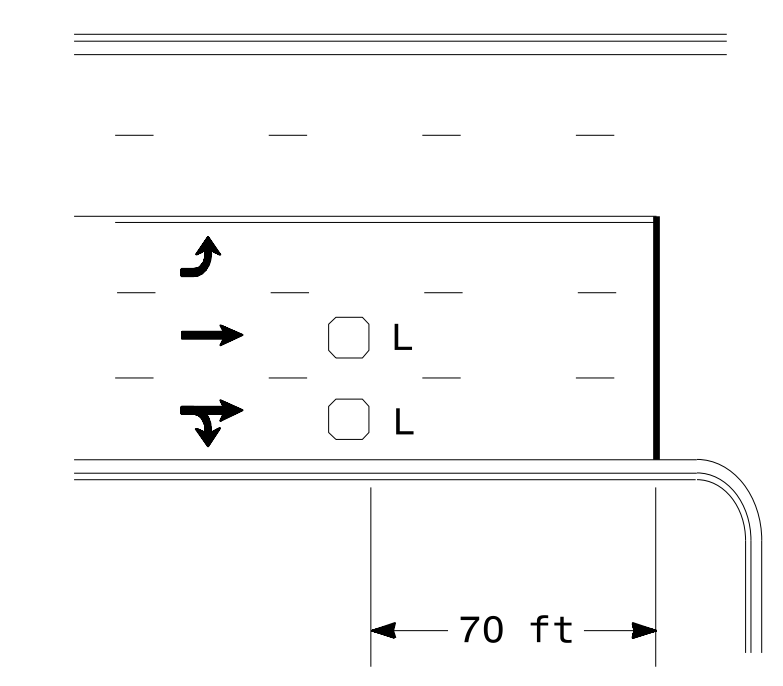


Speed Limit mph	D1 ft	D2 ft
40	250	80
45	300	90
50	355	100
55	420	110

L1 = 6ft X 6ft
Wired in series
L2 = 6ft X 6ft
Wired in series

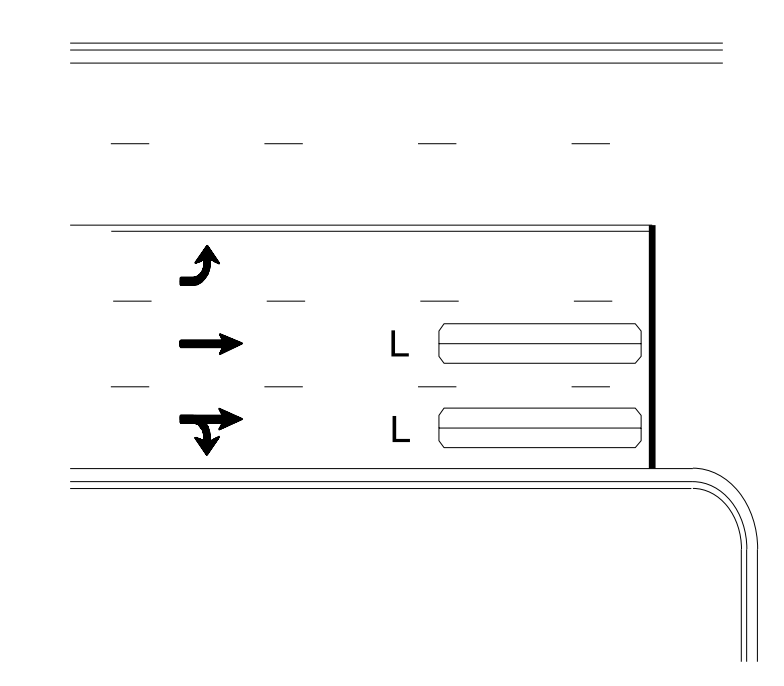
"Stretch" Operation

Low Speed Detection (≤35 mph)



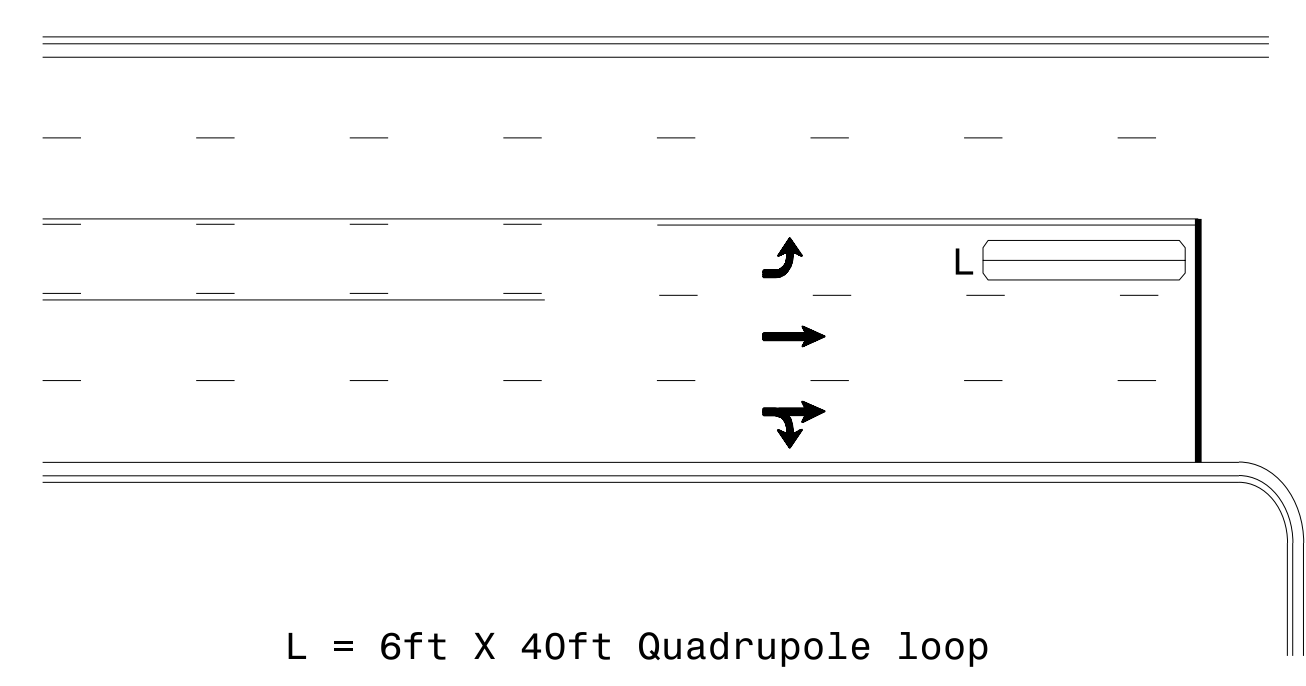
L = 6ft X 6ft
Wired in series

OR



L = 6ft X 40ft
Quadrupole loop, wired separately

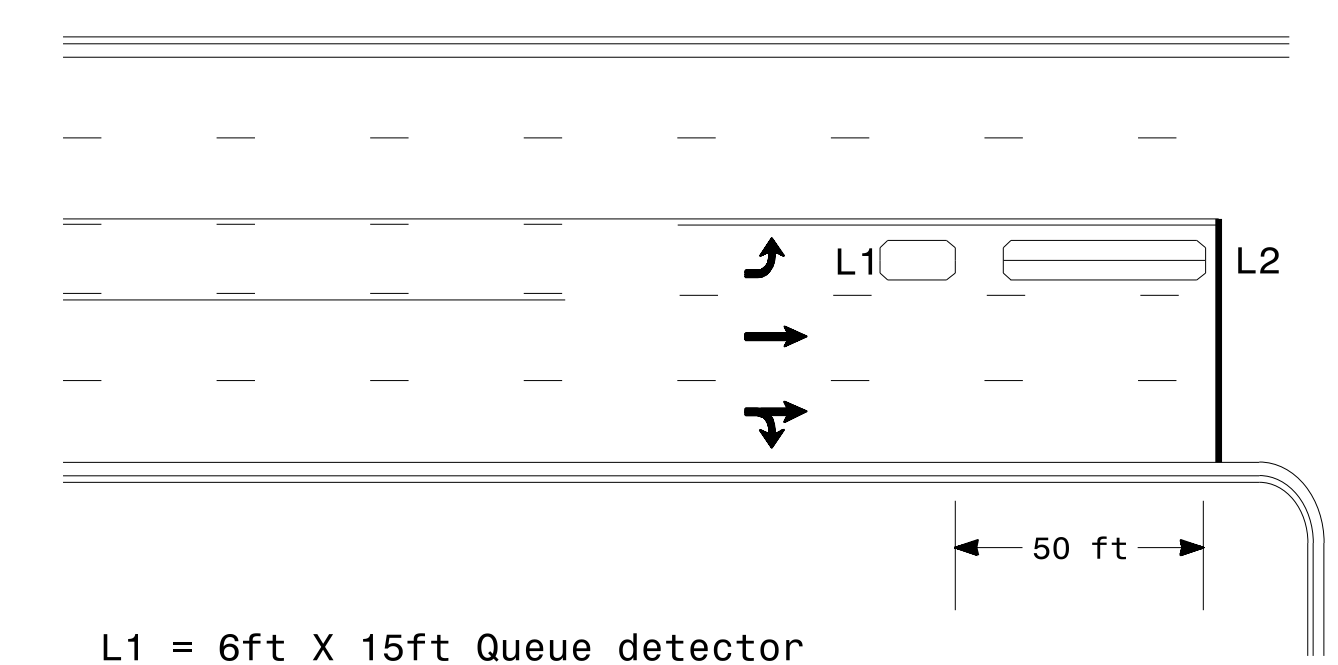
Left Turn Lane Detection



L = 6ft X 40ft Quadrupole loop

Presence Loop Detection

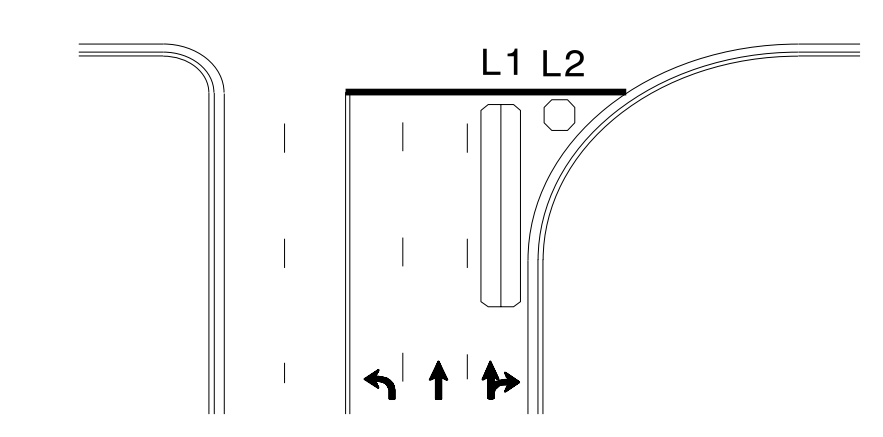
OR



L1 = 6ft X 15ft Queue detector
L2 = 6ft X 40ft Quadrupole loop

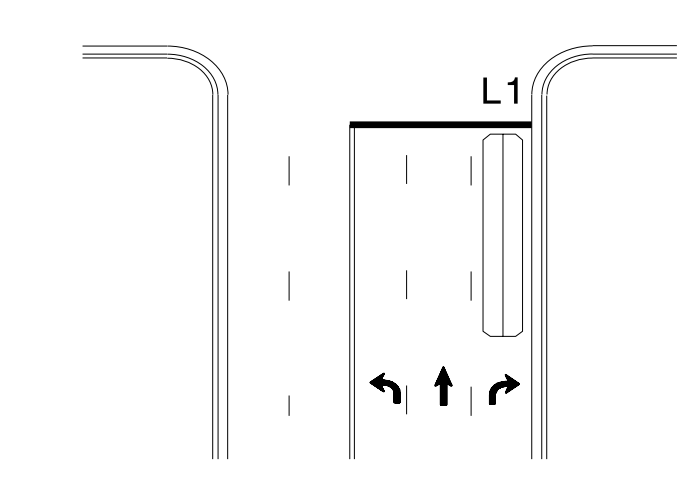
Queue Loop Detection

Right Turn Lane Detection

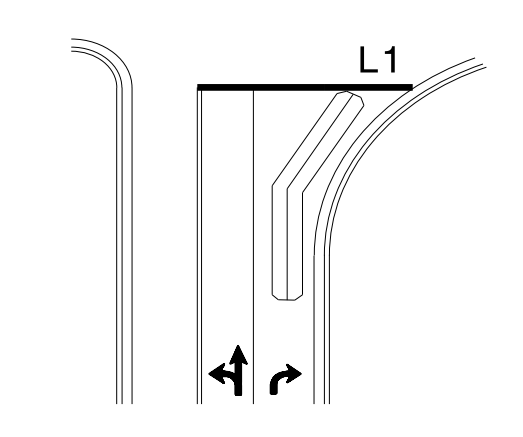


Shared Lane/
Wide Radius Turn

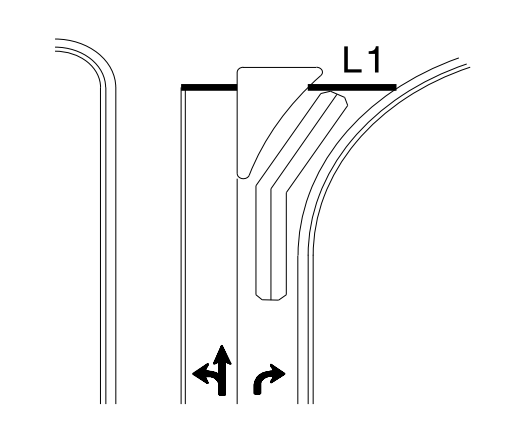
L1 = 6ft X 40ft Quadrupole loop
L2 = 6ft X 6ft [Minimum] Presence loop
Wired separately



Standard Turn

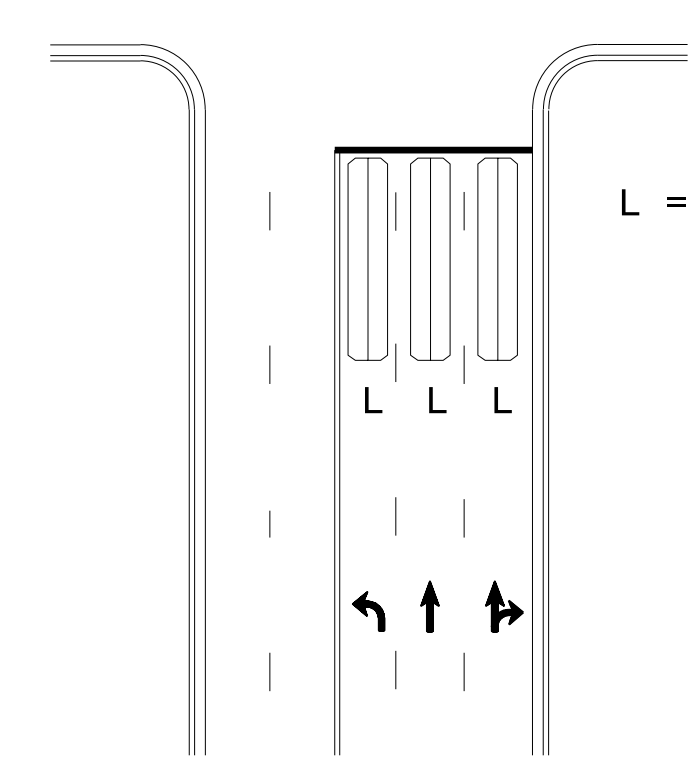


Wide Radius Turn



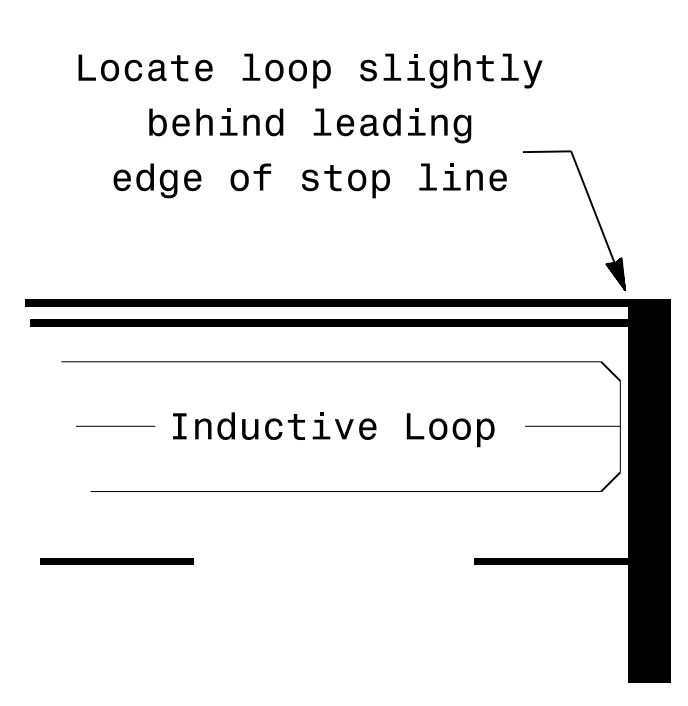
Channelized Turn

Side Street Detection



L = 6ft X 40ft
Quadrupole loop
Wired to separate
detectors/channels

Presence Loop Placement at Stop Lines



Locate loop slightly
behind leading
edge of stop line

Note:
Loop may be located in advance
of stop line under any of the
following conditions:
1) stop line is greater than 15'
from edge of intersecting
roadway
2) loop detects a permissive or
protected/permissive left turn
3) for an exclusive right turn
lane

Recommended Number of Turns

Single 6' X 6' loop
(when wired separately):

Length of Lead-in ft	Number of Turns
< 250	3
250-375	4
375-525	5
> 525	6

Quadrupole loops: Use 2-4-2 turns
6' X 15' Loops:
Lead-in < 150', use 2 turns
Lead-in > 150', use 3 turns

750 N. Greenfield Pkwy, Garner, NC 27529

Typical Signal Loop Locations

PLAN DATE: January 2015	REVIEWED BY: JPG
PREPARED BY: PLA	REVIEWED BY:
REVISIONS	INIT. DATE

SCALE: N/A

SEAL

1/30/2015

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 paalexander